

**Town of Pomfret
 Selectboard Special Meeting Agenda
 Town Offices
 5218 Pomfret Road, North Pomfret 05053
 September 12, 2023, 7:00 pm**

Zoom instructions below

Business Items	
1. Call to Order	7:00 pm
2. Agenda Review	
3. Public Comment	
4. Hearing re: Town Highway Access Permit (Mann)	7:05 pm
5. Hearing re: Town Highway Access Permit (White-Dechert)	8:05 pm
6. Meeting Wrap Up a. Correspondence b. Review of Assignments c. Agenda for Next Meeting	
7. Adjournment	
<i>Time frames are approximate. Members of the public wishing to attend for specific business items are encouraged to arrive before the time indicated.</i>	

Zoom Instructions

- Computer or Smartphone <https://zoom.us/j/95395079923?pwd=ZjBEed3ZuZWgvWmx2M0tpOE8zbjg2dz09>
- Mobile Phone +19292056099,,95395079923#,,#,306922#
- Landline or Mobile Phone (301) 715 8592, followed by Meeting ID 953 9507 9923 and Password 306922

TOWN OF POMFRET

APPLICATION FOR TOWN HIGHWAY ACCESS ROAD PERMIT
(Application fee of \$100 payable to Town of Pomfret)

Access Road Type (check one): Private Drive* Agricultural Forestry

Applicant Name: Andrew & Kathy Mann Phone: [REDACTED] Email: [REDACTED]

Applicant Mailing Address: PO Box 20 State: S.Pomfret Zip: 05067

(Complete only if Applicant is not the Landowner)

Landowner Name: _____ Phone: _____ Email: _____

Landowner Mailing Address: _____ State: _____ Zip: _____

The undersigned Applicant requests permission to develop, construct, or change the grade of the access road type indicated above.

Access Road Location: on the East side (cardinal direction) of Bartlett Brook Road (town highway name)
at approximately 5300' (feet) from the intersection of Pomfret Road (nearest town highway intersection).

Please provide a brief description of the work to be done: Widen existing drive on the north side in order to acquire a Driveway Access Permit conforming to existing specifications re width, 90 degree intersection, 1' slope away from town road for first 20' in order to improve driveway safety.

See attached sketch.

Width of driveway will be between 17' and 19'. Driveway will enter town road at 90 degrees and pitch away from town road 1' over the first 20' from the town road. See green rectangle on sketch.

The remainder of the existing driveway will be available for plowed snow and sand.

Site distances are at least 155' in both directions. Sand pile will be moved during construction.

Culvert at bottom of driveway will be extended as necessary.

(PROVIDE DETAILED SKETCH OF ACCESS LOCATION AND LAYOUT ABOVE)

The following conditions, standards and restrictions shall apply to the first 20 feet of a newly developed, constructed, or grade changed access road (measured from the edge of the traveled way) unless waived herein:

General Conditions

1. *Application Fee.* A \$100 non-refundable application fee shall be submitted before an application will be considered.
2. *Site Visit.* The Selectboard and Road Commissioner shall make a site visit prior to approving a permit.
3. *Final Site Inspection.* Upon completion of work, the Applicant or its agent shall notify the Town within ten days to arrange a final site inspection.
4. *Noncompliance.* Any work deemed not in compliance with the permit shall be corrected and reinspected for approval.
5. *Final Approval.* Work shall be approved by the Selectboard or its designee before any use of the access may be started.

* Each residence is limited to one driveway access.

Design Standards*

1. *Intersection Angle.* Access roads shall be constructed at a 90 degree angle to the town highway.
2. *Minimum Sight Distance.* Sight distances shall be at least as shown in the table below in both directions when viewed from a height of eye of 2.0 feet on the roadway to a height of eye of 3.5 feet on the drive at a point 15 feet back from edge of the traveled way (when entering upon paved portions of town highways) or 10 feet back from edge of the traveled way (when entering upon any other portion of a town highway). The “posted speed” shall be deemed to be 45 mph (when entering upon an unposted Class 2 highway) or 25 mph (when entering upon any other unposted highway).

Minimum Sight Distance

<i>Posted Speed (mph)</i>	<i>Minimum Sight Distance (feet)</i>
25	155
30	200
35	250
40	305
45	360

3. *Access Road Width.* Access roads shall be 16 feet wide and graded and sloped such that water from the access road does not enter the Town highway. It is recommended that the driveway or access road have a grade dropping six inches in 10 feet before extending either up or down slope.
4. *Culverts.* Culverts shall be installed if deemed necessary by the Selectboard or its designee and shall conform to standards described in Section 5.3(h) of the Pomfret Highway Ordinance.
5. *Vegetation.* Vegetation and trees shall be removed as necessary to provide for visibility and safety.

The undersigned Applicant agrees to adhere to the conditions, standards and restrictions forming a part of this permit, and understands that this permit, if issued, will be issued in accordance with 19 V.S.A. 1111 and the Pomfret Highway Ordinance and may be voided in the event of misrepresentation, substantial inaccuracy or failure to undertake development, construction, or changing the grade of the access road within one year of the date of approval. If Applicant is not the Landowner, Applicant has provided the Landowner with a copy of this completed Application and obtained the Landowner’s signature below.

Applicant Signature	Andrew Mann	2023-07-17
Applicant Signature	Applicant Printed Name	Date

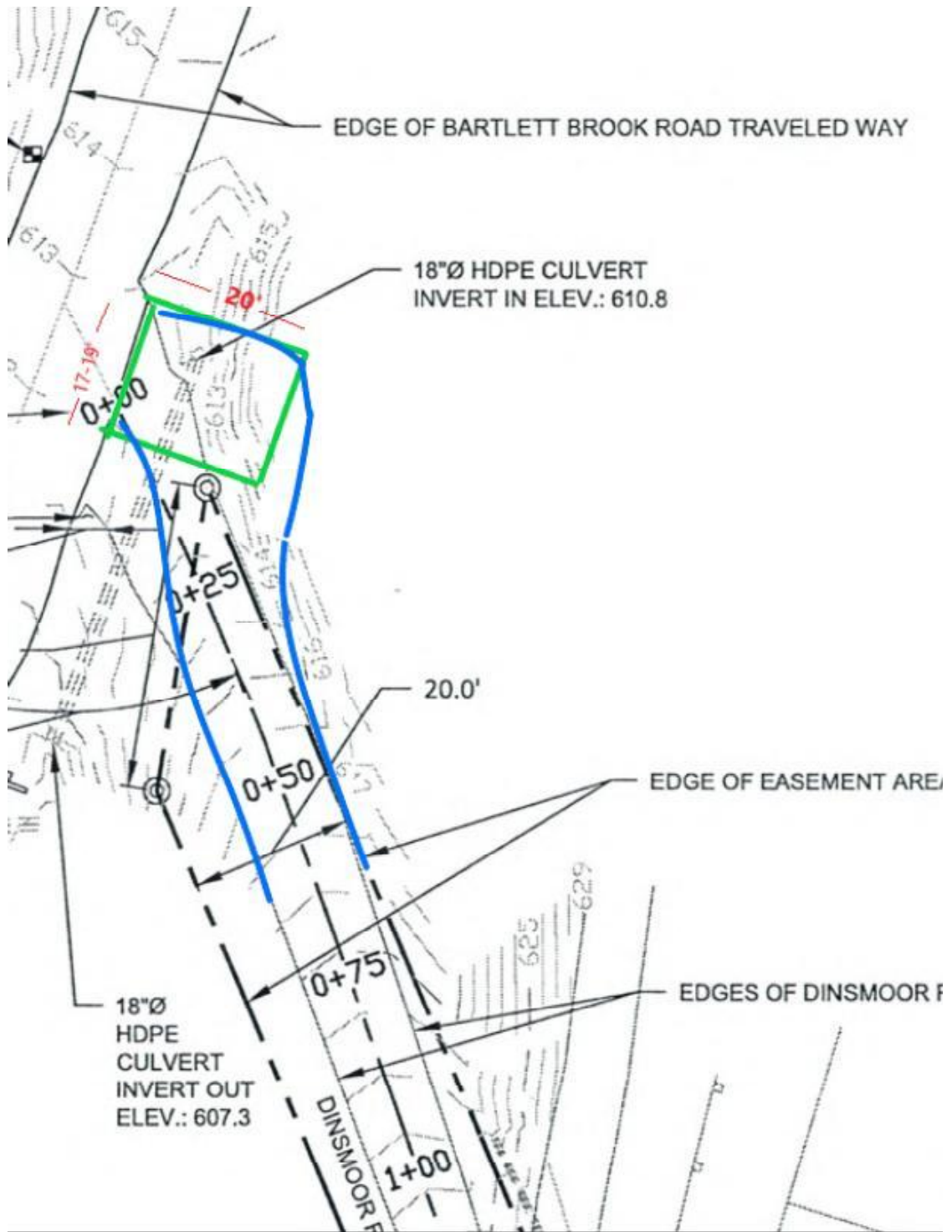
(Complete only if Applicant is not the Landowner)

The undersigned Landowner acknowledges they have received a copy of this completed Application.

Landowner Signature	Landowner Printed Name	Date
Landowner Signature	Landowner Printed Name	Date

[Remainder of page intentionally blank. Application form continues on next page.]

¹ Any of the standards listed herein may be waived or varied upon the Applicant’s request and following consideration by the Selectboard and Road Commissioner of public and private interests, topography, adequacy of highway design, ease of snow removal, drainage, and maintenance of safe conditions in all seasons for the traveling public.



Design Standards¹


1. *Intersection Angle.* Access roads shall be constructed at a 90 degree angle to the town highway.
2. *Minimum Sight Distance.* Sight distances shall be at least as shown in the table below in both directions when viewed from a height of eye of 2.0 feet on the roadway to a height of eye of 3.5 feet on the drive at a point 15 feet back from edge of the traveled way (when entering upon paved portions of town highways) or 10 feet back from edge of the traveled way (when entering upon any other portion of a town highway). The "posted speed" shall be deemed to be 45 mph (when entering upon an unposted Class 2 highway) or 25 mph (when entering upon any other unposted highway).

Minimum Sight Distance

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3. *Access Road Width.* Access roads shall be 16 feet wide and graded and sloped such that water from the access road does not enter the Town highway. It is recommended that the driveway or access road have a grade dropping six inches in 10 feet before extending either up or down slope.
4. *Culverts.* Culverts shall be installed if deemed necessary by the Selectboard or its designee and shall conform to standards described in Section 5.3(h) of the Pomfret Highway Ordinance.
5. *Vegetation.* Vegetation and trees shall be removed as necessary to provide for visibility and safety.

The undersigned Applicant agrees to adhere to the conditions, standards and restrictions forming a part of this permit, and understands that this permit, if issued, will be issued in accordance with 19 V.S.A. 1111 and the Pomfret Highway Ordinance and may be voided in the event of misrepresentation, substantial inaccuracy or failure to undertake development, construction, or changing the grade of the access road within one year of the date of approval. If Applicant is not the Landowner, Applicant has provided the Landowner with a copy of this completed Application and obtained the Landowner's signature below.


 Applicant Signature

Andrew Mann
 Applicant Printed Name

2023-07-17
 Date

(Complete only if Applicant is not the Landowner)

The undersigned Landowner acknowledges they have received a copy of this completed Application.

Landowner Signature

Landowner Printed Name

Date

[Remainder of page intentionally blank. Application form continues on next page.]

¹ Any of the standards listed herein may be waived or varied upon the Applicant's request and following consideration by the Selectboard and Road Commissioner of public and private interests, topography, adequacy of highway design, ease of snow removal, drainage, and maintenance of safe conditions in all seasons for the traveling public.

Better site plan illustration

Andrew Mann [REDACTED]

Sun, Sep 10, 2023 at 1:43 PM

To: "Brickner, Ben (Town)" <benjamin.brickner@pomfretvt.us>, "Chamberlin, Steve (Town)" <steve.chamberlin@pomfretvt.us>, "Emmons, Meg (Town)" <meg.emmons@pomfretvt.us>, "Grube, Emily (Town)" <Emily.Grube@pomfretvt.us>, "Peters, John (Town)" <john.peters@pomfretvt.us>
Cc: Joseph McLean <jmclean@firmspf.com>, Jason Crance <jason@crancelaw.com>, cteachout@doolaw.com, "Sander, Alison (ABS_GMail)" [REDACTED]

Selectboard,

I've attached a redrawn sketch to more accurately display the Mann site plan. This clarification is necessary as I am concerned I may have caused some confusion with how I originally displayed the expanded traveled way in the original plan.

In the attached sketch, the blue lines show what I intend for the new travelled way of the driveway. Note that the southern blue line is unchanged from the existing travelled way and has never changed. The travelled way will be expanded to the northern blue line.

The original sketch suggested that the southmost part of the existing travelled way would be subtracted, which is not and was never the intention (sorry for any confusion). The revised sketch shows accurately that nothing changes regarding the southern side of the travelled way. The only change is an increase to the width of the travelled way on the north side. Gains only for the driveway users, no losses.

Likewise the southern boundary of the driveway ROW, which includes the travelled way, remains unchanged. The thin red line depicts that southern ROW boundary and is extended from the southwest pin in the 1987 survey.

Ben asked at the site visit if I would extend the driveway ROW to include the part widened that would be outside the 1987 Easement Deed. Should the Mann plan prevail I think that would be a good idea, and I will follow up once things are resolved. The revised plan shows five red circles to indicate tentative boundary pins – and an "x" thru an existing pin which would be within a revised ROW and therefore eliminated – to display a revised ROW, which I will pursue with my neighbors if the Mann plan is approved and constructed.

Andy

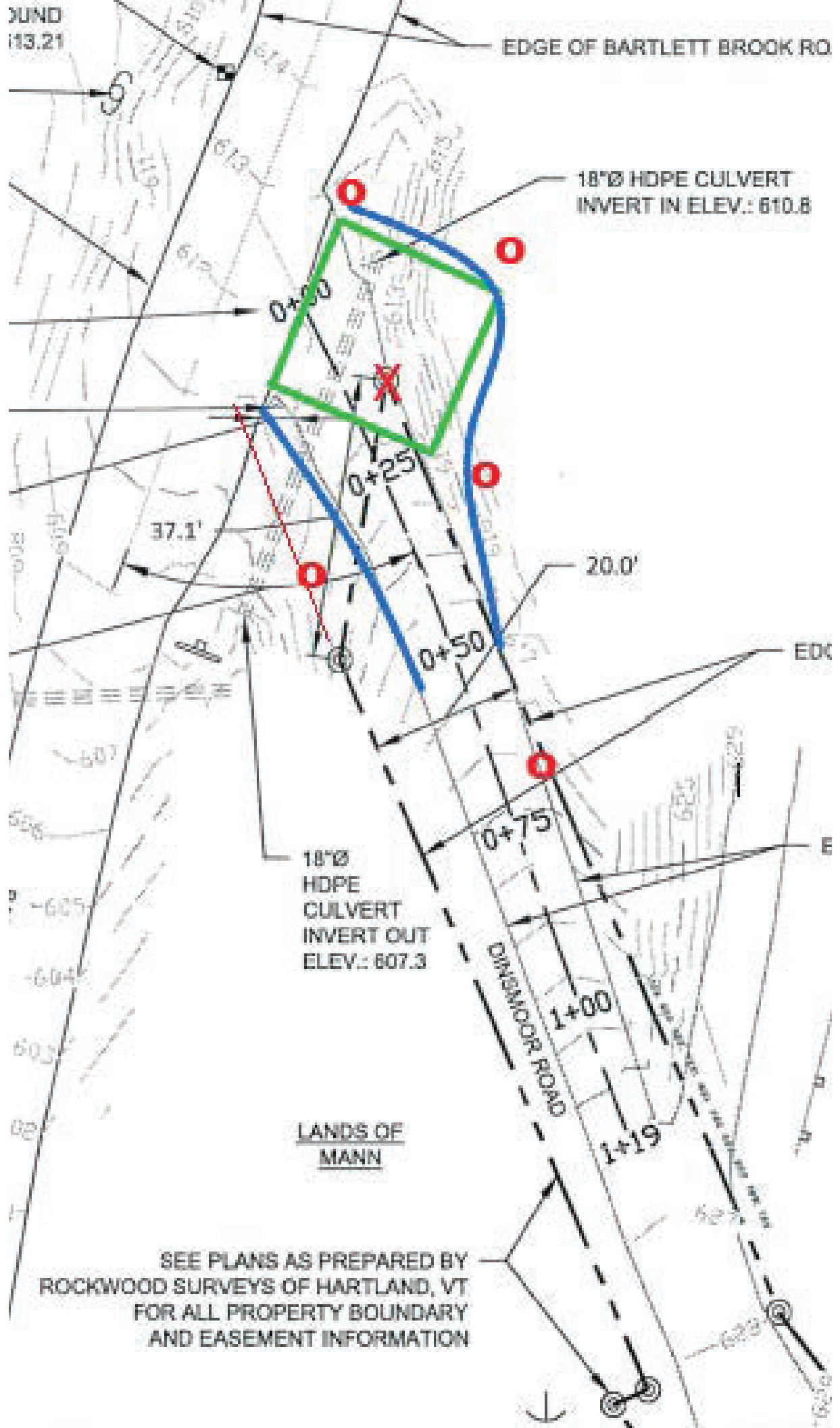
Andrew Mann

[REDACTED]
PO Box 20
S. Pomfret, VT 05067
[REDACTED]

XUND
113.21

EDGE OF BARTLETT BROOK RD

18"Ø HDPE CULVERT
INVERT IN ELEV.: 610.8



37.1'

20.0'

18"Ø
HDPE
CULVERT
INVERT OUT
ELEV.: 607.3

LANDS OF
MANN

SEE PLANS AS PREPARED BY
ROCKWOOD SURVEYS OF HARTLAND, VT
FOR ALL PROPERTY BOUNDARY
AND EASEMENT INFORMATION

TOWN OF POMFRET

APPLICATION FOR TOWN HIGHWAY ACCESS ROAD PERMIT
(Application fee of \$100 payable to Town of Pomfret)

Access Road Type (check one): Private Drive* Agricultural Forestry

Applicant Name: Hillary White and Philip Dechert Phone: [REDACTED] Email: [REDACTED]

Applicant Mailing Address: 195 Dinsmoor Road, So. Pomfret State: VT Zip: 05067

(Complete only if Applicant is not the Landowner)

Landowner Name: _____ Phone: _____ Email: _____

Landowner Mailing Address: _____ State: _____ Zip: _____

The undersigned Applicant requests permission to develop, construct, or regrade the access road type indicated above.

Access Road Location: on the east side (cardinal direction) of Bartlett Brook Road (town highway name)
at approximately 5,100 (feet) from the intersection of Pomfret Road (nearest town highway intersection).

Please provide a brief description of the work to be done: See Attached with Exhibits. This is not an application for a permit to create a new driveway access. It is an application to restore and improve an existing access located in the Town's ROW which has been utilized by the applicants for 35 years (+/-). Access has been rendered unsafe and the proposed plan remedies the current state of the access.

(PROVIDE DETAILED SKETCH OF ACCESS LOCATION AND LAYOUT ABOVE)

The following conditions, standards and restrictions shall apply to the first 20 feet of a newly developed, constructed, or regraded access road (measured from the edge of the traveled way) unless waived herein:

General Conditions

1. *Application Fee.* A \$100 non-refundable application fee shall be submitted before an application will be considered.
2. *Site Visit.* The Selectboard and Road Commissioner shall make a site visit prior to approving a permit.
3. *Final Site Inspection.* Upon completion of work, the Applicant or its agent shall notify the Town within ten days to arrange a final site inspection.
4. *Noncompliance.* Any work deemed not in compliance with the permit shall be corrected and reinspected for approval.
5. *Final Approval.* Work shall be approved by the Selectboard or its designee before any use of the access may be started.

* Each residence is limited to one driveway access.

Design Standards[†]

1. *Intersection Angle.* Access roads shall be constructed at a 90 degree angle to the town highway.
2. *Minimum Sight Distance.* Sight distances shall be at least as shown in the table below in both directions when viewed from a point 15 feet back from edge of the traveled way and at a height of eye of 3.5 feet on the drive to a height of eye of 2.0 feet on the roadway. The "posted speed" for unposted Class 3 highways shall be deemed to be 30 mph.

Minimum Sight Distance

<i>Posted Speed (mph)</i>	<i>Minimum Sight Distance (feet)</i>
25	155
30	200
35	250
40	305
45	360

3. *Access Road Width.* Access roads shall be 16 feet wide and graded and sloped such that water from the access road does not enter the Town highway. It is recommended that the driveway or access road have a grade dropping six inches in 10 feet before extending either up or down slope.
4. *Culverts.* Culverts shall be installed if deemed necessary by the Selectboard or its designee and shall conform to standards described in Section 5.3(h) of the Pomfret Highway Ordinance.
5. *Vegetation.* Vegetation and trees shall be removed as necessary to provide for visibility and safety.

The undersigned Applicant agrees to adhere to the conditions, standards and restrictions forming a part of this permit, and understands that this permit, if issued, will be issued in accordance with 19 V.S.A. 1111 and the Pomfret Highway Ordinance and may be voided in the event of misrepresentation, substantial inaccuracy or failure to undertake development, construction, or regrading of the access road within one year of the date of approval. If Applicant is not the Landowner, Applicant has provided the Landowner with a copy of this completed Application and obtained the Landowner's signature below.

x Philip Dechert Jr.
Hillary D. White
 Applicant Signature

PHILIP DECHERT JR.
HILLARY D. WHITE
 Applicant Printed Name

8-7-23
8-7-23
 Date

(Complete only if Applicant is not the Landowner)

The undersigned Landowner acknowledges they have received a copy of this completed Application.

 Landowner Signature

 Landowner Printed Name

 Date

[Remainder of page intentionally blank. Application form continues on next page.]

[†] Any of the standards listed herein may be waived or varied upon the Applicant's request and following consideration by the Selectboard and Road Commissioner of public and private interests, topography, adequacy of highway design, ease of snow removal, drainage, and maintenance of safe conditions in all seasons for the traveling public.

OFFICIAL USE ONLY

Application fee of \$100 received on _____, 20____ by _____.

Culvert Required (*min. 18-inch diameter*)? Yes No

Additional conditions, standards and restrictions; waivers of design standards (if any):

PERMIT APPROVED this _____ day of _____, 20____.

Road Commissioner

Selectboard Chair

FINAL APPROVAL. The work described in this permit has been constructed in accordance with the above conditions, standards and restrictions and is acceptable under State and local regulations.

Road Commissioner

Selectboard Chair

Date

Form adopted by the Pomfret Selectboard January 4, 2023. Supersedes all prior dated forms.

Attachment to Application for Town Highway Access Reconstruction/Improvement Road Permit
Town of Pomfret, Vermont

This attachment and the appended exhibits are submitted as part of Dr. White's Application for Town Highway Access Road Permit dated August 7, 2023.

Description of the topography of the Dinsmoor Rd easement and the Public ROW.

The area of travelled way subject to Dr. White's and Mr. Dechert's application allows access by Dr. White and Mr. Dechert via a deeded ROW (see Exhibit) to their dwelling, as well as other property owners uphill from the intersection of Dinsmoor Road and Bartlett Brook Road. As you enter Dinsmoor road from Bartlett Brook Road, there begins a steep ascent adjacent to (and north of) the north apron and a deep ravine adjacent to (and south of) the south apron onto Bartlett Brook Road. The turn into Dr. White's and Mr. Dechert's driveway, as it currently exists requires navigating a sharp and narrow angle with immediate/abrupt/steep/dangerous elevation gain and which can require multi-point turns or travel into oncoming traffic.

Dinsmoor Rd is a grandfathered and preexisting shared driveway.

Dr. White and Mr. Dechert are parties to the shared driveway, which was first set up by the original division of land into parcels by Ehrick Wood (1965/1966 Wood to Maynes to Griglik to White/Dechert to White) who conveyed a deeded ROW easement access to every parcel along Dinsmoor Rd (as recorded in the town's records). **Book 29, Page 225 (recorded 7/18/1966)**

As title owner of 195 Dinsmoor Rd, Dr. White and Mr. Dechert holds a right to use this 1966 ROW access easement described within the original warranty deed referenced above. This 1966 easement governs the parts of Dinsmoor Rd not specified in the 1987 easement (referenced below): the town ROW access below the 1987 easement, and the section of Dinsmoor Rd that is above Mr. and Mrs. Mann's lower parcel.

Dr. White and Mr. Dechert are also grantees of the 1987 Dinsmoor Rd ROW easement

In 1987 Mr. and Mrs. Mann conveyed an easement to Dr. White and Mr. Dechert which only covers a section of the lower portion of Dinsmoor Rd that runs along Mr. and Mrs. Mann's lower parcel. **Book 36, Page 137-39 (recorded 6/12/1987)**

This easement does not include the Public ROW, nor the bulk of the length of Dinsmoor Rd. The 1987 Dinsmoor Rd easement establishes, amongst other rights, that Dr. White and Mr. Dechert have the right to repair Dinsmoor Rd as necessary to protect their rights of access.

Safety issues for the town road and its intersection with Dinsmoor Rd.

In 2022, the downhill half of the rolling crown at the bottom of Dinsmoor Road was chiseled out to create an abrupt and steep grade change. Prior to 2022, access was had in the manner and along the route of the plan provided as part of this application. The 2022 change blocked access across the south apron. In 2023, a more substantial berm was constructed within a foot or so of the town travel way.

This "berm" has had the effect of obstructing access to Dinsmoor Road via the south apron and created significant safety issues, to Wit:

1. The berm blocks safe and convenient passage by emergency vehicles.
2. That berm will be in the path of the town plow lane in winter.

3. That berm prevented the Dinsmoor Road plow truck from being able to properly clear the apron lying in the highway right of way.
4. Delivery vehicles frequently cannot make the turn onto Dinsmoor Road, so they park at the bottom and block traffic on the town road.
5. Service vehicles frequently have to back up Dinsmoor Road or go past and turn around and approach the driveway from the north (this not being suitable for trucks with trailers or other oversized vehicles needing to access uphill landowners).
6. The berm forces oncoming traffic into the opposite lane of travel in order to access the driveway. Bartlett Brook Road is slightly curved and has an elevation gain right where Dinsmoor Road meets it, making it difficult to assess oncoming town highway traffic due to such a narrow, angular entryway.
7. The berm obstructs the sightlines of those persons entering Bartlett Brook Road from Dinsmoor Road. Drivers who cannot turn their neck far enough to the left are at high risk of not being able to see oncoming traffic on Bartlett Brook Road as it currently exists.
8. The berm has the potential to cause harmful water drainage onto the town road since there is no proper drainage swale.

The engineering plan presented by Dr. White and Mr. Dechert in the permit application provides a solution for all of these safety issues.

The engineering plan prepared by Ian McKensie, PE and the survey prepared by Tim Rockwood (licensed surveyor) that define the boundaries of the Dinsmoor Road easement demonstrate that Mr. McKenzie's plan will provide safe, convenient, and long-term access by property owners benefitted by the above referenced easement deeds all within the recorded easement boundaries.

The proposed work is predominantly within the Public ROW. As such, and in consultation with the Town, Dr. White and Mr. Dechert are submitting this permit application. In so doing, however, they reserve all available rights they have in law or equity to maintain safe access along the deeded right of way from Bartlett Brook Road to their property either as between Dr. White and Mr. Dechert and the Town and/or Dr. White and Mr. Dechert and Mr. and Mrs. Mann.

Dr. White's and Mr. Dechert's deeded right of way grants are appended as Exhibit 1.

Tim Rockwood's survey of Dinsmoor Road and Dr. White's and Mr. Dechert's property is appended as Exhibit 2.

Ian MacKenzie's Executive Summary of the proposed plan is appended as Exhibit 3.

Ian MacKenzie's Engineering plans are appended as Exhibit 4

Pictures of the berm and gravel pile as of July 29, 2023 are appended as Exhibit 5

1984 and 1986 Bruno and Associates survey of Mr. and Mrs. Mann's property as referenced in Tim Rockwood' Survey is appended as Exhibit 6.

Engineering plan highlights:

1. The current 1-foot turning radius will become a 15-foot turning radius.
2. The current 46-degree angle between town road and driveway centerlines will become a 75-degree angle within the first 15-feet. (A minimum of 60-degrees is preferable.)

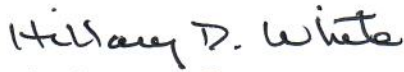

3. An 18-inch precast concrete headwall near the culvert outflow will stabilize the south apron and help prevent erosion, control drainage, etc.
4. The town shoulder will be restored.
5. A drainage swale with appropriate specifications will be added to improve drainage and protect the town highway.
6. Specifications for materials deemed suitable by the engineer will be confirmed prior to placement. Construction with these materials will be a significant improvement for stabilization and drainage:
 - a. Subbase material. An 18-inch minimum layer of 3-inch minus dense grade crushed stone (VAOT 704.06) will be used for the subbase material and crowned.
 - b. Surface material. A 4-inch-thick surface fine (VAOT 704.05B) will be used for the surface material.
7. Culverts: The culvert at the bottom of the driveway and parallel to the town road was recently replaced (~2021-2022) as allowed by the town.
8. Vegetation: as needed with approval by the town.

Dr. White and Mr. Dechert's engineer carefully considered a north apron expansion but rejected the same since a more northerly access will not meet engineering standards, will prevent reasonable ingress and egress on Dinsmoor Road, and will prevent gain of momentum under icy conditions (i.e., vehicles will not have sufficient travel momentum to ascend Dinsmoor Road if icy).

Dr. White and Mr. Dechert have standing to submit this permit application request: "The nature of a right of way is such that travel or usage as access is encouraged." *Patch v. Baird*, 140 Vt. 60, 64, 435 A.2d 690, 692 (1981) "[T]he owner of the easement is entitled to a convenient, reasonable, and accessible way, having regard to the interest and convenience of the owner of the land as well as their own." *VTRE Invs. v. MontChilly, Inc.*, 2020 VT 77, ¶ 23 (references omitted). A grantee of an easement "has the same right of property therein as he has in land. He is entitled to his specific property and cannot be divested thereof without his consent." *La Fleur v. Zelenko*, 101 Vt. 64, 72, 141 A. 603, 606 (1928). The grantee of a right of way has the duty and the right "to construct or keep in repair the way granted." *Walker v. Pierce*, 38 Vt. 94, 98 (1865).

Respectfully submitted,

Dr. Hillary White
Mr. Philip Dechert, Jr.

Row Wood to Maynes

B29 P225

1966

29:225

KNOW ALL MEN BY THESE PRESENTS

THAT We, Ehrick J. Wood and Blanche R. Wood, husband and wife, of Pomfret in the County of Windsor and State of Vermont Grantors, in the consideration of One Dollar and other valuable considerations paid to our full satisfaction by William H. Maynes and Shannon N. Maynes, husband and wife, of Hastings-on-Hudson in the County of Westchester and State of New York Grantees, by these presents, do freely GIVE, GRANT, SELL, CONVEY AND CONFIRM unto the said Grantees William H. Maynes and Shannon N. Maynes, husband and wife, as tenants by the entirety, and their heirs and assigns forever, a certain rights-of-way in Pomfret in the County of Windsor and State of Vermont, described as follows, viz:

1. Being a right-of-way for ingress and egress located on the southerly side of the highway leading to the property of the grantors hereunder and leading in a southeasterly direction to property this day conveyed by the grantees hereunder to the grantors hereunder, said right-of-way being for ingress and egress and said right-of-way being located between property now or formerly of the grantors hereunder and one Raymond Rose and said right of way to be twenty-five feet in width and to connect to an old logging road on the property above referred to which was conveyed by the grantees hereunder to the grantors hereunder of even date and about to be recorded in the land records of the Town of Pomfret. Said right-of-way is for the use of the grantees, their heirs or assigns, or others who may desire to use said right-of-way in the event the grantees hereunder sub-divide property located southeasterly of the right-of-way herein described.

? Spooner Row
< Spooner Rd ROW

2. Being a right-of-way for electric poles & electric lines, said right-of-way being located on the 50 foot right-of-way leading from the dirt road going by property of the grantors hereunder to property conveyed to the grantees hereunder in September, 1965, said deed having already been recorded in the Pomfret Land Records and said right-of-way therein described being enlarged in order to include electric lines and pole lines; and the further right to connect to the electric lines now located on the grantors property.

< Dinsmoor Rd ROW

3. It is also an express condition of this deed and obligation of the grantors hereunder that the grantors will construct a passable highway for automobiles over the right-of-way first above described herein at the sole expense of the said grantors, and it is further understood said passable highway shall be constructed within one year from the date hereof.

autos

Exhibit 1

ROW Wood to Maynes

ROW Mann to White/Dechert

TO HAVE AND TO HOLD said granted premises, with all the privileges and appurtenances thereof, to the said Grantees William H. Maynes, and Shannon N. Maynes, husband and wife, and their heirs and assigns, to their own use and behoof forever; And we the said Grantors Ehrick J. Wood and Blanche R. Wood, husband and wife, for ourselves and our heirs, executors and administrators, do covenant with the said Grantees William H. Maynes and Shannon N. Maynes, husband and wife, and their heirs and assigns, that until the sealing of these presents we are the sole owners of the premises, and have good right and title to convey the same in manner aforesaid, that they are FREE FROM EVERY ENCUMBRANCE; And we do hereby engage to WARRANT AND DEFEND the same against all lawful claims whatever, IN WITNESS WHEREOF, We hereunto set our hands and seals this 8th day of July A.D. 1966. IN PRESENCE OF

Patricia West
Franklin S. Billings, Jr. Ehrick J. Wood
Blanche R. Wood

STATE OF VERMONT
COUNTY WINDSOR

SS. At Woodstock this 8th day of July A. D. 1966 Ehrick J. Wood and Blanche R. Wood personally appeared, and they acknowledged this instrument, by them sealed and subscribed, to be their free act and deed.
Before me, Franklin S. Billings, Jr., Notary Public

The undersigned Raymond and Mary Rose join in this conveyance in order to release and convey the right to erect electric poles and lines as described in paragraph 2 above since they presently own title to the land under said right of way. At Woodstock in the County of Windsor and State of Vermont, we hereunto set our hands and seals this 21 day of July A. D. 1966.
IN PRESENCE OF:

Franklin S. Billings, Jr. Raymond Rose
Pauline B. Maynes Mary Rose

STATE OF VERMONT
WINDSOR COUNTY, SS.

At Woodstock in said County this 21 day of July A. D. 1966, the above named Raymond Rose and Mary Rose personally appeared and they acknowledged this instrument, by them sealed and subscribed, to be their free act and deed.
Before me, Franklin S. Billings, Jr., Notary Public

Pomfret Town Clerk's Office: Received for record on July 23, A. D. 1966 at 12:30 P.M. Attest:

Barbara Payne
Town Clerk

Know all Men by these Presents

That We, Andrew A. Mann and Kathy Jean Mann

36:137-9

of Pomfret in the County of Windsor
and State of Vermont Grantors, in the consideration of
One dollar and Other Good and Valuable Consideration Dollars
paid to our full satisfaction by
Philip Dechert, Jr. and Hillary W. Dechert

of Pomfret in the County of Windsor
and State of Vermont Grantees, have REMISED, RELEASED,
AND FOREVER QUITCLAIMED unto the said
Philip Dechert, Jr. and Hillary W. Dechert, and their

all right and title which heirs or assigns,
we the said Andrew A. Mann and Kathy Jean Mann

easement and right-of-way or our heirs have in, and to a
certain piece of land in Pomfret in the
County of Windsor and State of Vermont, described as
follows, viz:

- Being an easement and right of way in common with grantors, their heirs and assigns, for ingress and egress to the land and premises of grantees, across land and premises conveyed to the grantors by the following deeds:
1. Warranty deed of Raymond L. Collamore and Alice G. Collamore dated November 26, 1984 and recorded in Book 34 at Page 281 of the Town of Pomfret Land Records, and
 2. Warranty deed of Kurt A. Vreeland and Patricia A. Vreeland dated November 12, 1984 and recorded in Book 34 at page 282 of the Town of Pomfret Land Records.

The location of this right of way is shown on a plan prepared by Bruno Associates, Inc. P.C. entitled "Proposed Right-of-way across lands of Andrew and Kathy Mann in Pomfret, Vermont" dated December 1986, scale 1" = 40', and is described thereon as follows:

Commencing at a 5/8" rebar set in the ground near the intersection of Town Highway #39 and an existing drive, and running S 22 degrees 10' E 132.4' to a rebar set in the ground; thence proceeding S 36 degrees 12' E 67.8' to a rebar set in the ground; thence turning an angle to the left and proceeding N 58 degrees 43' E 10.0' to a point; thence turning an angle to the right and proceeding S 28 degrees 37' E 246.4' to a 1/2" iron pipe; thence turning an angle to the right and proceeding N 86 degrees 27' W 41.3' to a point; thence turning an angle to the right and proceeding N 28 degrees 37' W 223.0' to a point; thence proceeding N 36 degrees 12' W 68.2' to a point; thence turning an angle to the right and proceeding N 67 degrees 50' E 5.0' to a point; thence turning an angle to the left and proceeding N 22 degrees 10' W 103.6' to a point; thence turning an angle to the right and proceeding N 10 degrees 27' E 37.1' to a 5/8" rebar, being the point and place of beginning.

Also conveyed herein is an easement in common with Grantors, their heirs and assigns, for electric and telephone utility purposes, to be established and located identically to the utility easement being conveyed by instrument of these grantors dated December 12, 1986 to Central Vermont Public Service Corporation and New England Telephone and Telegraph Company.

continued....

36:138

Manns Row
to Phil/Hilary

36:138

Both grantors and grantees intend by delivery and acceptance of this deed to extinguish all other and prior easements and rights-of-way of record and in fact which may cross the grantors' land for the benefit of grantees' land, including, without limitation, those rights-of-way created by deeds of Ehrick J. Wood and Blanche R. Wood to William H. Maynes and Shannon H. Maynes dated July 8, 1966 and recorded in Book 29 at page 225 of the Town of Pomfret Land Records, and dated September 24, 1965 and recorded in Book 29 at page 110 of the Town of Pomfret Land Records.

The rights and interests conveyed herein are subject to rights of mortgages under now existing mortgages.

Grantees, their heirs and assigns, shall notify grantors in advance of any cutting of trees within the herein conveyed easement area, except in case of emergency.

tree cutting

Grantees, for themselves and their heirs and assigns, waive any claim for contribution in the event that grantors or their family members wish to connect to the utility lines established within this easement. Grantors reserve that right to connect to said utility lines for themselves and their family members, but not for other assigns.

util lines

Reference may be had to the above-mentioned deeds and plan and to all prior deeds in the Town of Pomfret Land Records for a more particular description of the rights herein conveyed.

Grantees, their heirs and assigns, are further conveyed a reasonable right if necessary, to repair and maintain their driveway, and for such specific purpose may exceed the bounds of the herein-conveyed easement only with Grantor's permission, such permission, not to be unreasonably withheld. It is specifically agreed and understood between the parties hereto, their heirs and assigns and successors, that such right shall be limited to the necessity of repair in the event of washout, substantial deterioration and the like, necessitating substantial repairs or reinstallation of the subject driveway. If this right is exercised, Grantees, their heirs and assigns shall repair at their expense any damage thereby caused to Grantor's premises, and shall restore Grantor's premises to as near its original condition as possible.

driveway repair, maint.

washout deterioration, like

continued....

TO HAVE AND TO HOLD all right and title in and to said quit-claimed premises, with the appurtenances thereof, to the said Philip Dechert, Jr. and Hillary W. Dechert, and their

heirs and assigns forever.

36:139

AND FURTHERMORE we the said Andrew A. Mann and Kathy Jean Mann

do for ourselves and our heirs, executors and administrators, covenant with the said Philip Dechert, Jr. and Hillary W. Dechert, and their

heirs and assigns, that from and after the ensembling of these presents the said

Andrew A. Mann and Kathy Jean Mann

will have and claim no right, in, or to the said quit-claimed premises, except as set forth herein

IN WITNESS WHEREOF, we hereunto set our hand and seal this day of A.D. 19

In Presence of

Muriel Norman witness to AAM & KJM

Andrew A. Mann L.S.

Hildi S. Kemison witness to AAM & KJM

Kathy Jean Mann L.S.

[Signature] witness to PD & HWD

Philip Dechert, Jr. L.S.

Noema Martin witness to PD & HWD

Hillary W. Dechert by Philip Dechert, Jr. her attorney in fact L.S.

STATE OF VERMONT, Windsor County ss.

At 12th day of June A. D. 1987 this

Andrew A. Mann and Kathy Jean Mann

personally appeared, and they acknowledged this instrument, by them sealed and subscribed, to be their free act and deed.

Before me Muriel Norman Notary Public (Title)

STATE OF VERMONT WINDSOR COUNTY, SS At Budgewater

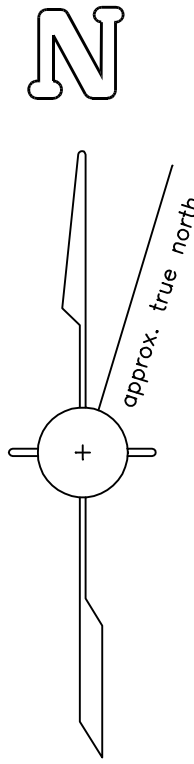
this 10 day of June 1987 PHILIP DECHERT, JR. and HILLARY W. DECHERT personally appeared, and they acknowledged this instrument by them sealed and subscribed, to be their free act and deed.

Before me Noema Martin Town Clerk Notary Public

Pomfret Town Clerk's Office: Received for record on June 12, A.D. 1987 at 12:40 P.M. A true copy. Attest: Hazel B. Huntington Town Clerk

Vermont Property Transfer Tax 32 V.S.A. Chap. 231 ACKNOWLEDGMENT Return Recd. by the Public-Board of Health Cert. Recd. by the Land Use & Development Plans Act Cert.

Magnetic North as rotated to the Bruno 1986 Right-of-Way Plan recorded in Pomfret Map Book 2, Page 137



NOTE: The Original 50 ft wide ROW for this location was along an "Old Dirt Road". This Road is shown on the 1984 Bruno Survey. The 1986 ROW deed drawn from the 1986 Bruno Survey replaced only the portion shown on this plan of this 50 ft ROW. Should ownership of Lot 3918 extend into the Public Way to the centerline of the highway, it would follow that the original 50' wide ROW also extends. This would occur from the 37.1 ft line westerly potential to the centerline of Bartlett Brook Road.

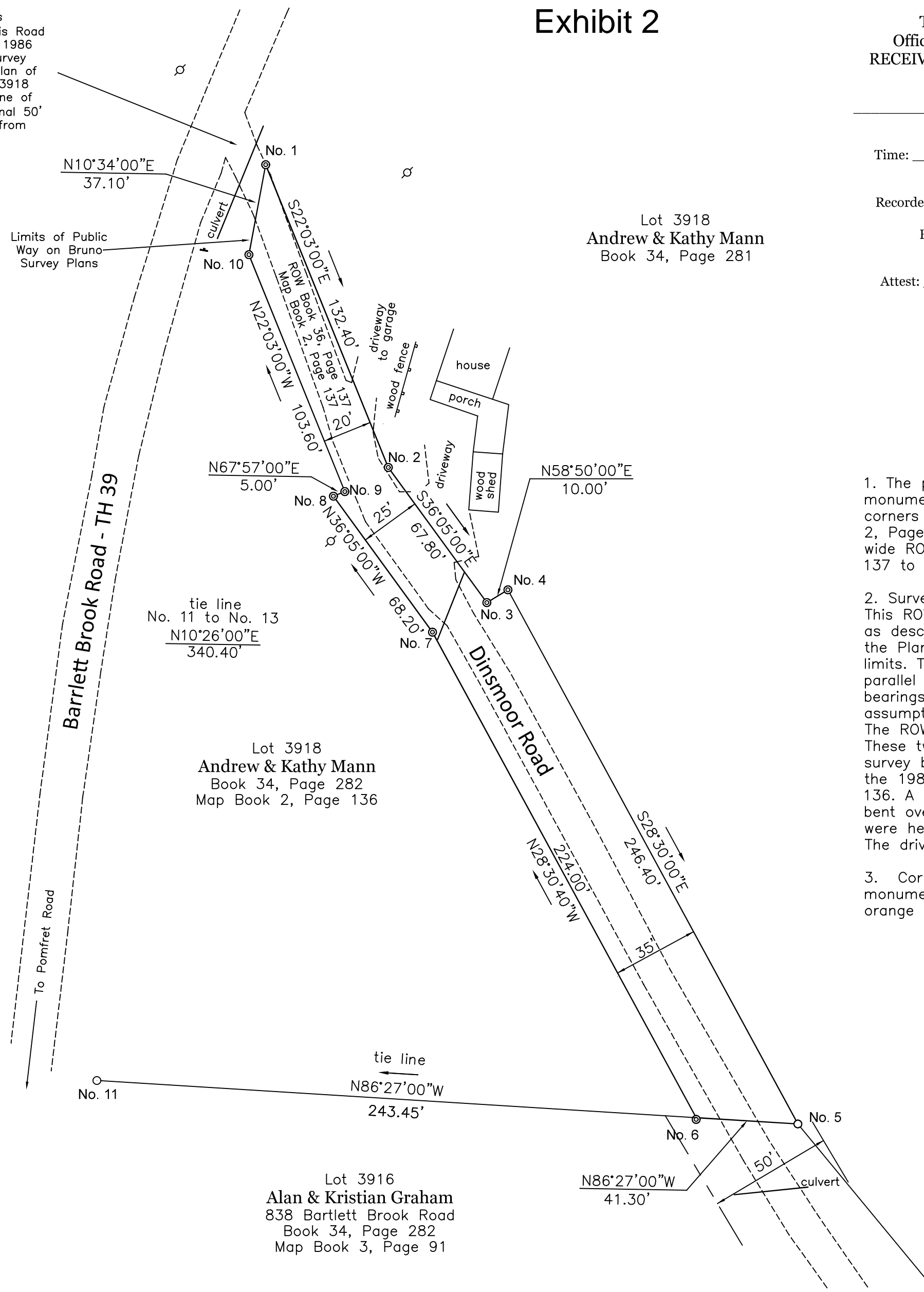
Monument Descriptions

- No. 1 SET a 5/8"x12" blue Rebar with Orange Plastic Cap down 1" in the gravel driveway, firm and straight
- No. 2 SET a 5/8"x36" blue Rebar with Orange Plastic Cap and 3" of reveal firm and straight
- No. 3 SET a 5/8"x36" blue Rebar with Orange Plastic Cap and 3" of reveal firm and straight, replacing a FOUND 1/2" rebar bend and in poor condition
- No. 4 SET a 5/8"x36" blue Rebar with Orange Plastic Cap and 6" of reveal firm and straight
- No. 5 FOUND a 1/2" orange Rebar with old flagging in a small rock pike and 12" of reveal firm and straight
- No. 6 SET a 5/8"x42" blue Rebar with Orange Plastic Cap and 15" of reveal firm and straight, driven to refusal
- No. 7 SET a 5/8"x42" blue Rebar with Orange Plastic Cap and 9" of reveal firm and straight
- No. 8 SET a 5/8"x36" blue Rebar with Orange Plastic Cap and 8" of reveal firm and straight
- No. 9 SET a 5/8"x36" blue Rebar with Orange Plastic Cap and 6" of reveal firm and straight
- No. 10 SET a 5/8"x42" blue Rebar with Orange Plastic Cap and 10" of reveal firm and straight
- No. 11 FOUND a 1/2" Rebar with old flagging and 14" of reveal firm and straight

Legend

- FOUND Rebar or Iron Pipe ○ as described
- SET Capped Rebar ● as described
- Right-of-Way Limits ————
- Edge of Gravel - - - - -
- Utility Pole ⚡
- Deed Reference Book, Page
- Plan Reference Map Book, Page

Exhibit 2

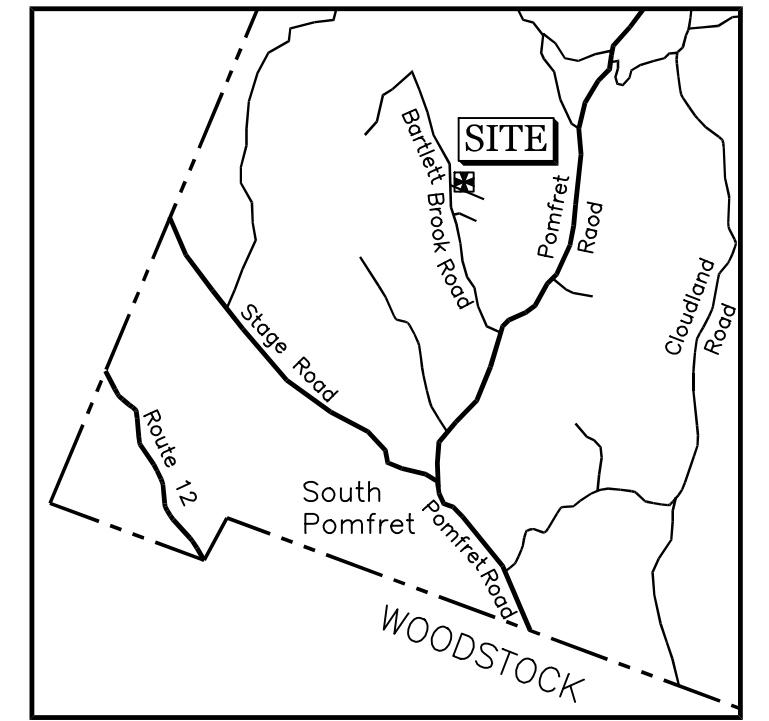


Lot 3918
Andrew & Kathy Mann
Book 34, Page 281

Lot 3918
Andrew & Kathy Mann
Book 34, Page 282
Map Book 2, Page 136

Lot 3916
Alan & Kristian Graham
838 Bartlett Brook Road
Book 34, Page 282
Map Book 3, Page 91

Town of Pomfret
Office of the Town Clerk
RECEIVED FOR RECORDING
on _____, 2023
at _____
Time: _____
Recorded as Plan No. _____
File No. _____
Attest: _____
Town Clerk



LOCUS
n.t.s.

Survey Notes and Report

1. The purpose of this Right-of-Way (ROW) retracement Plan is to identify and monument the limits of the ROW. The limits of this ROW are monumented at the corners as deeded and shown on 1986 Bruno Associates Survey recorded in Book 2, Page 137. The ROW as Proposed by Bruno Associates was to be replaced a 50 ft wide ROW over the Old Dirt Road. The ROW deed is found in Book 36 on Pages 137 to 139.

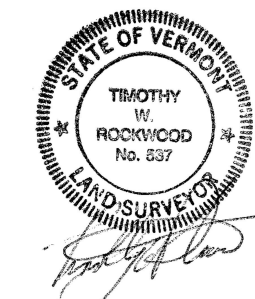
2. Surveyor's Report:
This ROW Survey is a retracement of the Deeded ROW to Lots 3918 and 3918-B as described in the Deed recorded in Book 36, Pages 137-139 and shown on the Plan recorded in Map Book 2, Page 137. No changes were made to the ROW limits. The ROW description has a 0.92 ft mathematical error. ROW distances and parallel lines were held to meet the ROW's intent. The slight differences in bearings along the ROW limits is likely due to the 0.92 ft math error. No assumptions or changes were made to adjust for the math error.
The ROW location was established using found monuments No. 5 and No. 11. These two rebars were found to be consistent with both the 1984 boundary survey by Bruno Associates for lands to be conveyed by Vreeland to Mann and the 1986 ROW Survey. This Boundary Survey is recorded in Map Book 2, Page 136. A rebar found at No. 3 was consistent with the 1986 ROW limits but was bent over and replaced. The bearings on both Bruno Plans are consistent and were held on this ROW retracement plan.
The driveway travel way shown are the current driveway's gravel limits.

3. Corner monuments noted as SET were placed on May 15, 2023. All monument dimensions are outside diameters. Monuments are capped with a 1" orange plastic cap and painted blue.

Easement and Right-of-Way
across Parcel 3918
Philip Dechert, Jr. and Hillary W. Dechert
195 Dinsmoor Road
South Pomfret, VT
Deed References: Book 36, Pages 137
ROW Plan Reference: Map Book 2, Page 137

Land Owner of Record
Parcel 3918
Andrew A. Mann & Kathy Jean Mann
Dinsmoor Road
South Pomfret, VT
Deed References: Book 34, Pages 281 and Book 34, Page 282
Plan Reference: Map Slide ??

I hereby certify that to the best of my belief and knowledge a Vermont Rural Class Survey was completed on the Right-of-Way limits shown in accordance with the requirements of 27 VSA 1403(b)-(8).



TIMOTHY W. ROCKWOOD
LLS NO. 537

Retracement Survey of the Easement
and Right-of-Way across Lot 3918

for
Philip & Hillary Dechert

Dinsmoor Road, Pomfret, Windsor County, Vermont



Project No. 1027-1 Date: June 19, 2023

Rockwood Land Services, LLC

PO BOX 347 Scale: 1" = 40' (802)436-1039
HARTLAND, VT 05048 rockls@vermontel.net

June 13, 2023

Town of Pomfret Selectboard
5218 Pomfret Road
Pomfret, VT 05053

Exhibit 3

Re: Dinsmoor Road Shared Access
Proposed Improvement Project

Members of the Selectboard:

The following is an executive summary accompanying a site plan proposal for improvements to the shared access of Dinsmoor Road to Bartlett Brook Road for Hillary White. Applicant resides at 195 Dinsmoor Road. The intent of this effort is to restore access by emergency and utility vehicles that is not possible with the current condition of the access.

Existing Conditions and General Site Information

- Dinsmoor Road is a private road with shared access by multiple separate residential properties from the Town Highway Bartlett Brook Road (BBR).
- As provided by the applicant, a 1966 50-foot-wide right-of-way “for highway passable by automobiles” was established for Dinsmoor Road and the separate residential properties that do not have road frontage on BBR for ingress and egress.
- As provided by the applicant, a 1987 Dinsmoor Rd easement survey (Mann as grantor, White and neighbors as grantees) defines easement boundaries as starting at the eastern edge of the town right-of-way (per survey locations) and follows along Mann’s lower parcel only; it ends where Mann’s upper parcel begins.
- Licensed survey information for the shared access easement was provided digitally by Tim Rockwood, Rockwood Land Services, LLC of Hartland, Vermont. Rockwood is a VT Licensed Land Surveyor. Rockwood returned to set boundary pins on-site that reflect the locations shown on the basemap prepared with this application.
- Topographic survey was performed by Ian MacKenzie, PE with relation to common benchmarks with Rockwood’s field efforts. Existing grades and easement (as provided by Rockwood) are shown on the site plans (Sheet C1.00).
- Existing grade centerline profile (Sheet C1.20) is based on the center of the current hardpacked shared access surface. A relative station reference of 1+00 was set arbitrarily at the edge of the hardpacked shared access and the edge of the traveled way of BBR. Dinsmoor Road makes slight sweeping approach to BBR within the existing easement area.
- Dinsmoor Road meets BBR with essentially no interior radius (apron) for north bound traffic to negotiate turning onto Dinsmoor Road without entering the opposite lane at the far edge of BBR traveled way. **1-foot turning radius** is shown for illustrative purposes.
- Dinsmoor Road meets BBR on the inner portion of a sweeping, curved portion of BBR.
- Current Dinsmoor Road centerline meets BBR at an approx. **46° angle**.
- Nominal 1”+/- depression exists at the edge of BBR as the Dinsmoor Road shared access centerline is upgradient at 5-8% within the initial 10’ rounding length from the edge of BBR. BBR grade is approx. 7% in the area of the Dinsmoor Road intersection. Dinsmoor Road grades proceed upgradient at varying 8.9 to 14.6 percentages within 115’ of BBR.

Analysis and Design Effort

- Options for re-grading Dinsmoor Road to meet current VTrans B-71B (standard for private driveways of three residential properties or more) were attempted but not feasible due to the existing horizontal easement constraints and site topography. VTrans B-71B standards considered not feasible included a min. 60° angle for private drives within Town Highway ROW, 5" depression for centerline grades within a 20' rounding length with a 15% max. slope for centerline grade within 25' of the vertical intersection, and a 30' edge of drive radius for apron width.
- Option for re-grading Dinsmoor Road to improve initial depression and rounding length vertical geometry was attempted by carrying a centerline slope of 16% from the first residential drive to BBR, but was not reasonably feasible given the existing excessive slopes and exorbitant costs that would be associated for minor benefit within the initial 20'.
- Option for reasonable improvement to allow proper traffic movements onto Dinsmoor Road from BBR are proposed as shown on Sheet C2.00.

Design Proposal

Key site geometry and features within the shared access improvement plans (Sheet C2.00, C2.10 and C2.20) include:

- Construct shared access apron on southern edge to include a **15' radius** with necessary associated **grading and drainage** within the shared access easement area.
- Provide **75° angle of initial 15' of proposed Dinsmoor Road centerline at intersection with BBR.**
- Accommodate south apron construction and grades by proposed **precast concrete headwall** to be placed on the outlet of the existing culvert within the Town R.O.W. to stabilize the south apron.
- Headwalls (rubble masonry/ concrete) are common practice for culverts within Town R.O.W. as understood so long as they do not interfere with normal Town Highway grading efforts.
- Proposed headwall allows grading of the apron to provide a sweeping upgradient edge of traveled way and **stone-lined drainage swale** for a stable shoulder to Dinsmoor Road while ensuring drainage is properly directed to the BBR roadside ditch.

Notables

- This shared access project process including existing conditions summary, analysis and design effort, and improvement proposal were presented by myself on May 24th, 2023 to the residential property grantor for the 1987 Dinsmoor Rd easement for shared access.

Closing

This shared access improvement project is provided as a reasonable option for the involved parties to obtain proper and safe vehicular movements onto Dinsmoor Road from Bartlett Brook Road.

Permission for placement of headwall within Town R.O.W. is requested by this submittal for the betterment of all users of Dinsmoor Road and Bartlett Brook Road. Do not hesitate to contact me to discuss or review proposal particulars.

Sincerely,

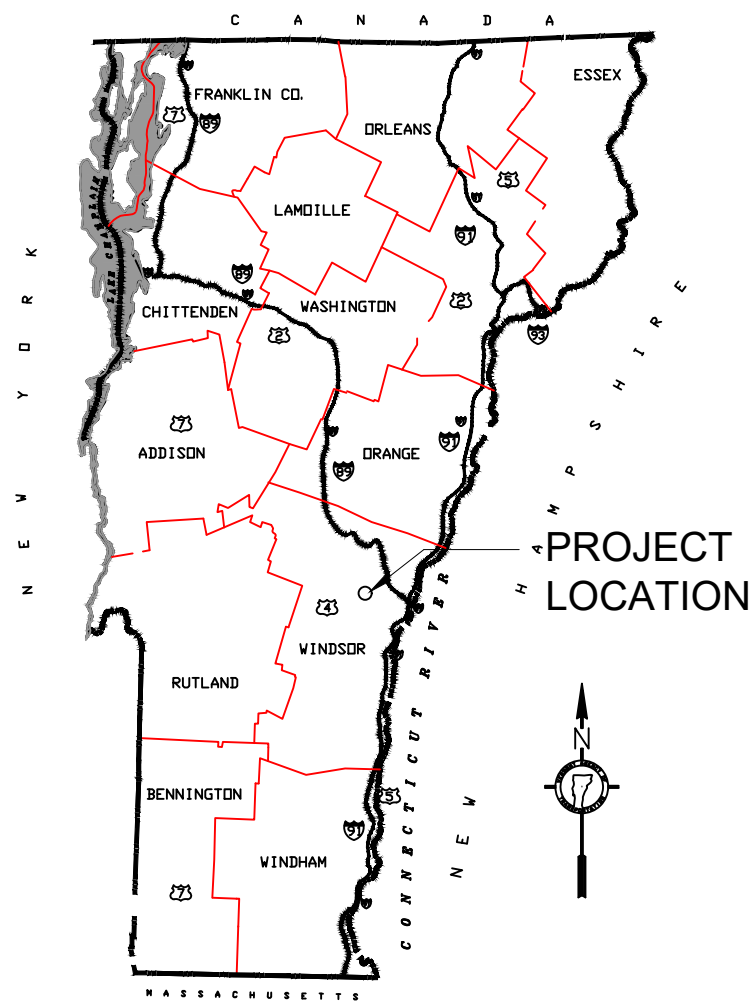
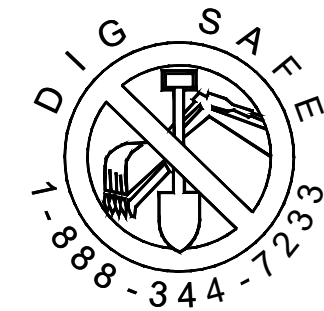


Ian S. MacKenzie, PE



Project: Dinsmoor Road Access to Bartlett Brook Road
Client: Hillary White
Location: Bartlett Brook Road
Windsor County
Pomfret, Vermont

Exhibit 4



PREPARED BY:

Mackenzie
Engineering and Construction







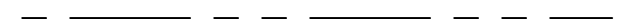



1313 VT Route 110
South Royalton, VT 05068
Email: ian@mack-eng.com

DATE: JUNE 13, 2023

GENERAL NOTES:

1. RIGHT OF WAY AND PROPERTY BOUNDARY INFORMATION IS AS PROVIDED BY ROCKWOOD LAND SERVICES, LLC. OF HARTLAND, VERMONT.
2. PROPERTY LINES, EASEMENTS AND OTHER REAL PROPERTY DESCRIPTIONS PROVIDED IN THIS PLAN SET ARE FOR THE USE OF SHARED ACCESS IMPROVEMENT PLANNING ONLY. THEY DO NOT DEFINE LEGAL RIGHTS OR MEET LEGAL REQUIREMENTS FOR A LAND SURVEY AS DESCRIBED IN 26 V.S.A. s-2502(4), AND SHALL NOT BE USED IN LIEU OF A SURVEY AS THE BASIS OF ANY LAND TRANSFER OR ESTABLISHMENT OF ANY PROPERTY RIGHT.
3. IAN S. MACKENZIE, PE DOES NOT TAKE WITNESS TO PROPERTY BOUNDARY INFORMATION.
4. EXISTING CONTOUR AND ELEVATION INFORMATION AS SHOWN IS RELATIVE AND NOT BASED ON ACTUAL MEAN SEA LEVEL DATUM.
5. PROPOSED INFORMATION AND FEATURES INDICATED BY FRAMED TEXT
6. PLAN NORTH ORIENTATION IS APPROXIMATE BASED ON TRUE NORTH.
7. ALTERATION OF ANY INFORMATION AS DEPICTED WITHIN THESE PLANS IS PROHIBITED AND SHALL RENDER THESE DOCUMENTS VOID.
8. AGENT/OWNER/CLIENT IS RESPONSIBLE FOR OBTAINING AND MAINTAINING COMPLIANCE WITH ALL LIFE, SAFETY, OSHA, AND BUILDING PERMITS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION AS NECESSARY.
9. CONTRACTOR(S) IS(ARE) RESPONSIBLE TO PERFORM ALL SERVICES IN ACCORDANCE TO ALL APPLICABLE STATE AND FEDERAL WORK SAFETY CODES.
10. ALL EARTHWORK SHALL BE PERFORMED IN ACCORDANCE TO THE VERMONT LOW RISK SITE HANDBOOK. CONTRACTOR IS RESPONSIBLE FOR ALL EROSION AND SEDIMENT CONTROL.
11. LOCATIONS, ELEVATIONS AND ORIENTATION OF ALL PHYSICAL ITEMS IN WORK AREA SHALL BE VERIFIED AND FURTHER IDENTIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. ANY DISCREPANCY IN ELEVATIONS OR DIMENSIONS SHOWN ON THE PLANS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER.
12. CONTRACTOR SHALL BE RESPONSIBLE TO PROPERLY DIGSAFE THE WORK SITE AND ALSO LOCATE EXISTING PRIVATE & NON-MEMBER UNDERGROUND UTILITIES THAT MAY NOT BE LOCATED BY A DIGSAFE EFFORT.
13. CONTRACTOR IS RESPONSIBLE FOR BEING FAMILIAR WITH ALL CURRENT APPLICABLE VERMONT ENVIRONMENTAL PROTECTION RULE CHAPTERS.
14. BURIED AND OVERHEAD UTILITIES WERE NOT LOCATED.
15. BASE SURVEY, PHYSICAL AND TOPOGRAPHIC INFORMATION IS PER TOPOGRAPHIC SURVEY AS PERFORMED BY MACKENZIE ENGINEERING AND CONSTRUCTION, LLC FOR THE PURPOSE OF SHARED ACCESS IMPROVEMENT ONLY.
16. TOPOGRAPHIC SURVEY WAS PERFORMED WITH LEICA TOTAL STATION.

LEGEND

- CONTOUR – MAJOR 
- CONTOUR – MINOR 
- CONTOUR – PROPOSED  1376
- EDGE OF STREAM 
- TOP OF BANK 
- EXISTING CULVERT 
- EASEMENT 
- EXISTING EDGE OF GRAVEL 
- EXISTING TREELINE 
- DIRECTION OF STREAM FLOW 

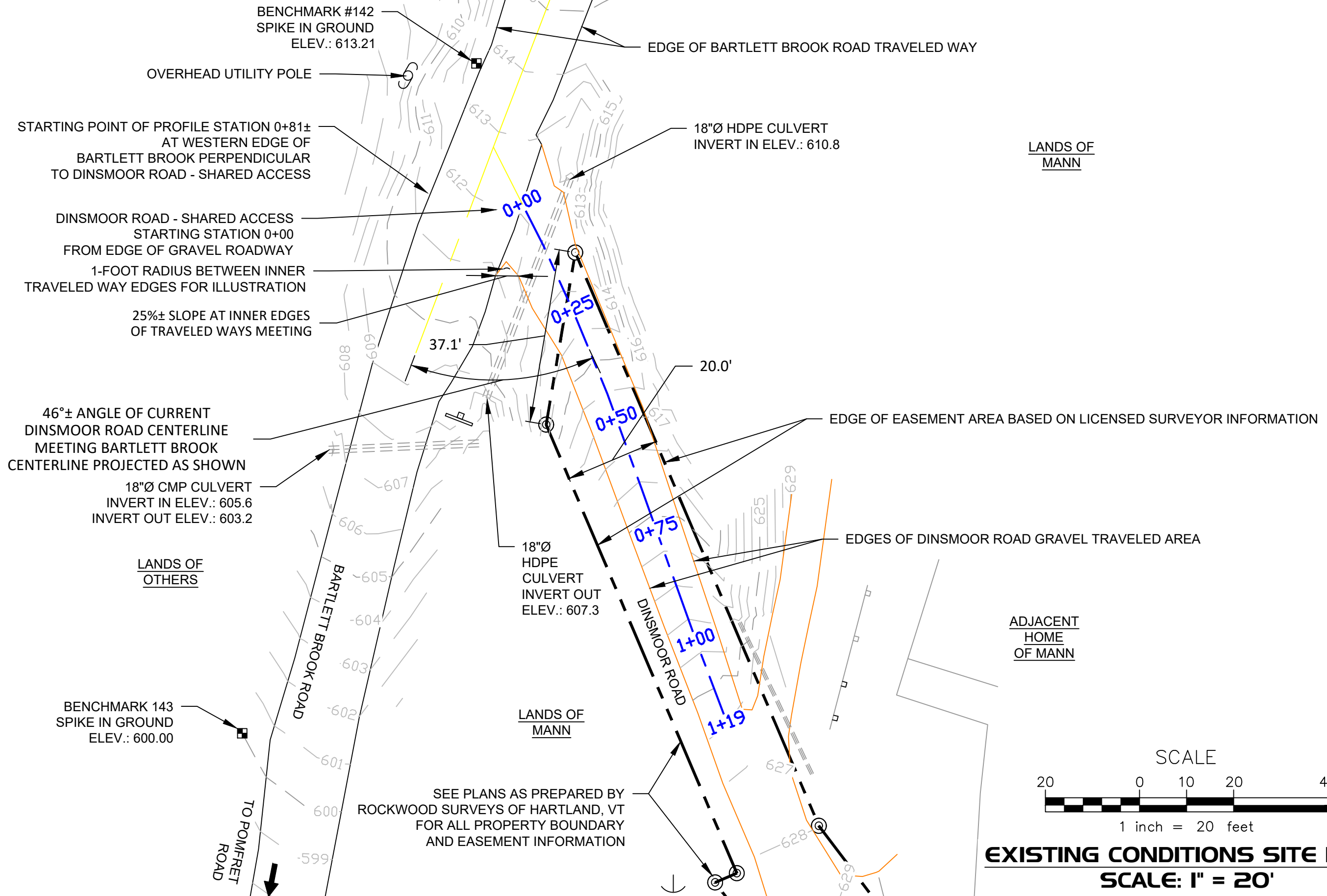
**PROJECT NOTES
AND LEGEND SHEET**

PROJECT TITLE:	SHARED ACCESS ENTRANCE TO BARTLETT BROOK
SHEET TITLE:	NOTES AND LEGEND
PROJECT NO.:	2304
SHEET NO.:	CO.00
DATE:	6-12-23

DINSMOOR ROAD
SHARED ACCESS
Bartlett Brook
Pomfret, Vermont



MACKENZIE
Engineering and Construction
1313 VT Route 110, So. Royalton, VT 05068
Mobile No.: 802 - 356 - 5617
Email: ian@mack-eng.com



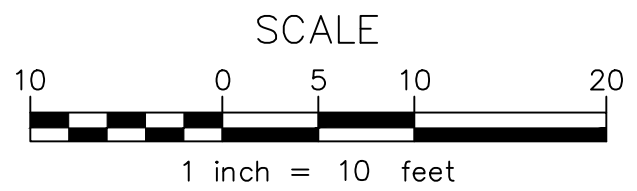
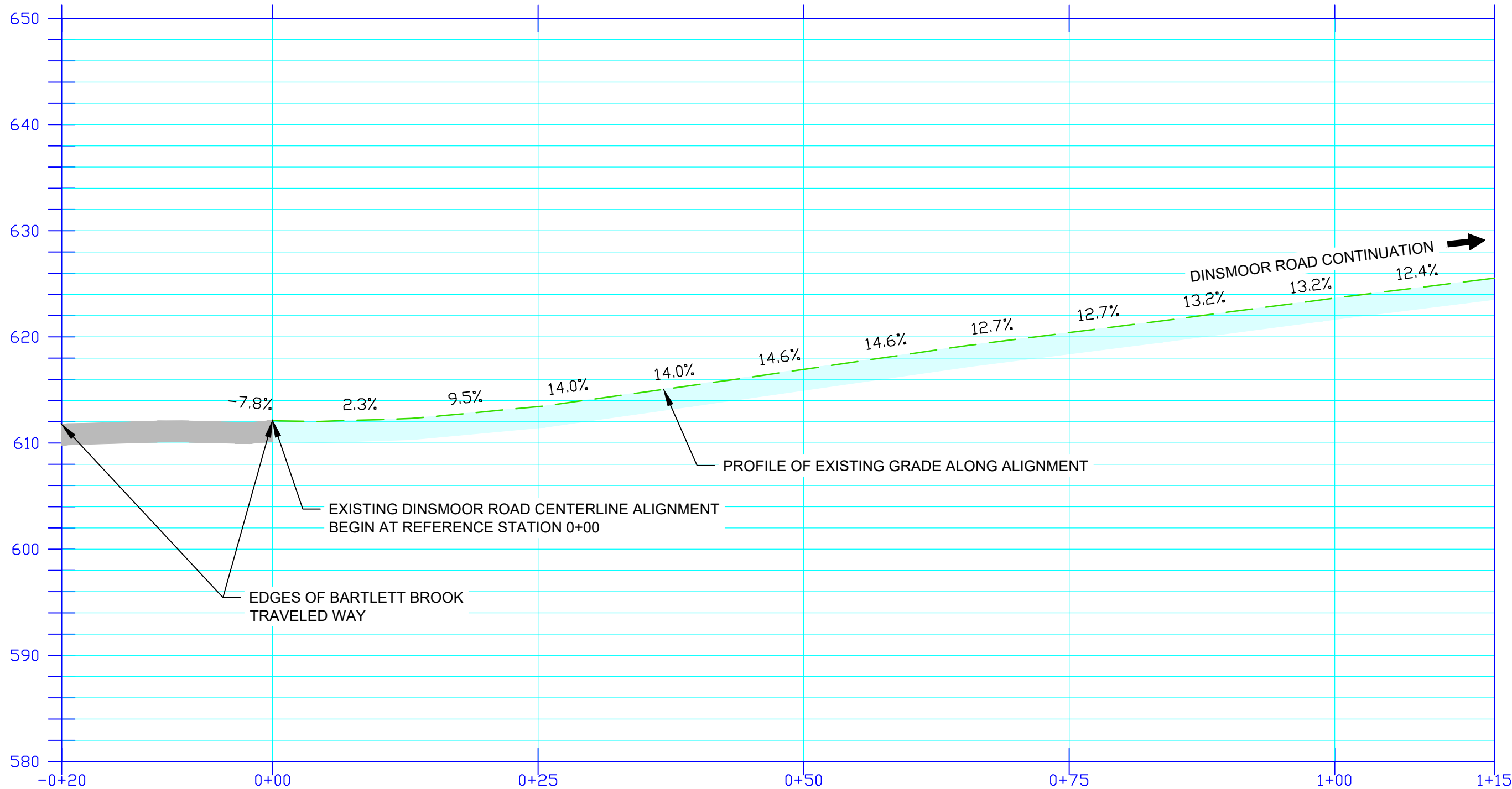
EXISTING CONDITIONS SITE PLAN
SCALE: 1" = 20'

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SHEET TITLE:	EXISTING CONDITIONS SITE PLAN	SHEET NO.:	C1.00
		DATE:	5-8-23

DINSMOOR ROAD
SHARED ACCESS
Bartlett Brook
Pomfret, Vermont



MACKENZIE
Engineering and Construction
1313 VT Route 110, So. Royalton, VT 05068
Mobile No.: 802 - 356 - 5617
Email: ian@mack-eng.com



DINSMOOR ROAD
EXISTING CENTERLINE ALIGNMENT PROFILE
SCALE: 1" = 10' (HORIZONTAL & VERTICAL)

PROJECT TITLE:	SHARED ACCESS ENTRANCE TO BARTLETT BROOK
SHEET TITLE:	EXISTING CENTERLINE PROFILE
PROJECT NO.:	2304
SHEET NO.:	C1.10
DATE:	6-12-23

DINSMOOR ROAD
 SHARED ACCESS
 Bartlett Brook
 Pomfret, Vermont



MACKENZIE
 Engineering and Construction
 1313 VT Route 110, So. Royalton, VT 05068
 Mobile No.: 802 - 356 - 5617
 Email: ian@mack-eng.com

BENCHMARK #142
SPIKE IN GROUND
ELEV.: 613.21

PROPOSED CENTERLINE ALIGNMENT
STARTING POINT OF STATION 1+00

LANDS OF
OTHERS

75°± ANGLE OF PROPOSED
DINSMOOR ROAD CENTERLINE
MEETING BARTLETT BROOK
CENTERLINE PROJECT AS SHOWN

TAPER ROAD SHOULDER
GRADE TO APRON,
TRANSITION GRADES
AS SHOWN

INSTALL STONE-LINED
DRAINAGE SWALE
AT SOUTH SIDE
OF SHARED ACCESS

RESTORE TOWN
ROAD SHOULDER

18"Ø PRECAST
CONCRETE HEADWALL
SEE DETAIL SHEET C2.20

FINISH GRADING (TYP)

15' RADIUS AS
SHOWN PROPOSED

EASEMENT BOUNDARY
MONUMENT TO BE SET
BY LIC. SURVEYOR

LANDS
OF MANN

LIMITS OF DISTURBANCE
TO PROVIDE PROPER FINISH SURFACE GRADING AND TRANSITIONS AS NECESSARY

MAINTAIN NORTH DRIVEWAY EDGE

EASEMENT BOUNDARY MONUMENT
TO BE SET BY LICENSED SURVEYOR

MAINTAIN NORTH DRAINAGE DITCH

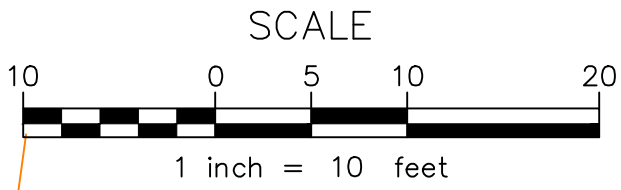
PROVIDE 16% MAX GRADE ALONG CENTRAL PORTION OF SHARED ACCESS

WIDENING OF SHARED ACCESS TO MEET 15' RADIUS

EXTENT OF SHARED ACCESS BOX-CUT AND GRADING
SEE SHARED ACCESS SECTION DETAIL, SHEET C2.20

MAINTAIN WORK WITHIN EASEMENT BOUNDARY
SEE PLANS AS PREPARED BY LICENSED SURVEYOR

LANDS OF
MANN



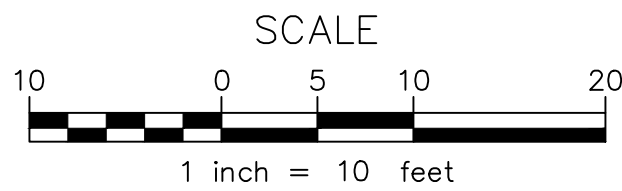
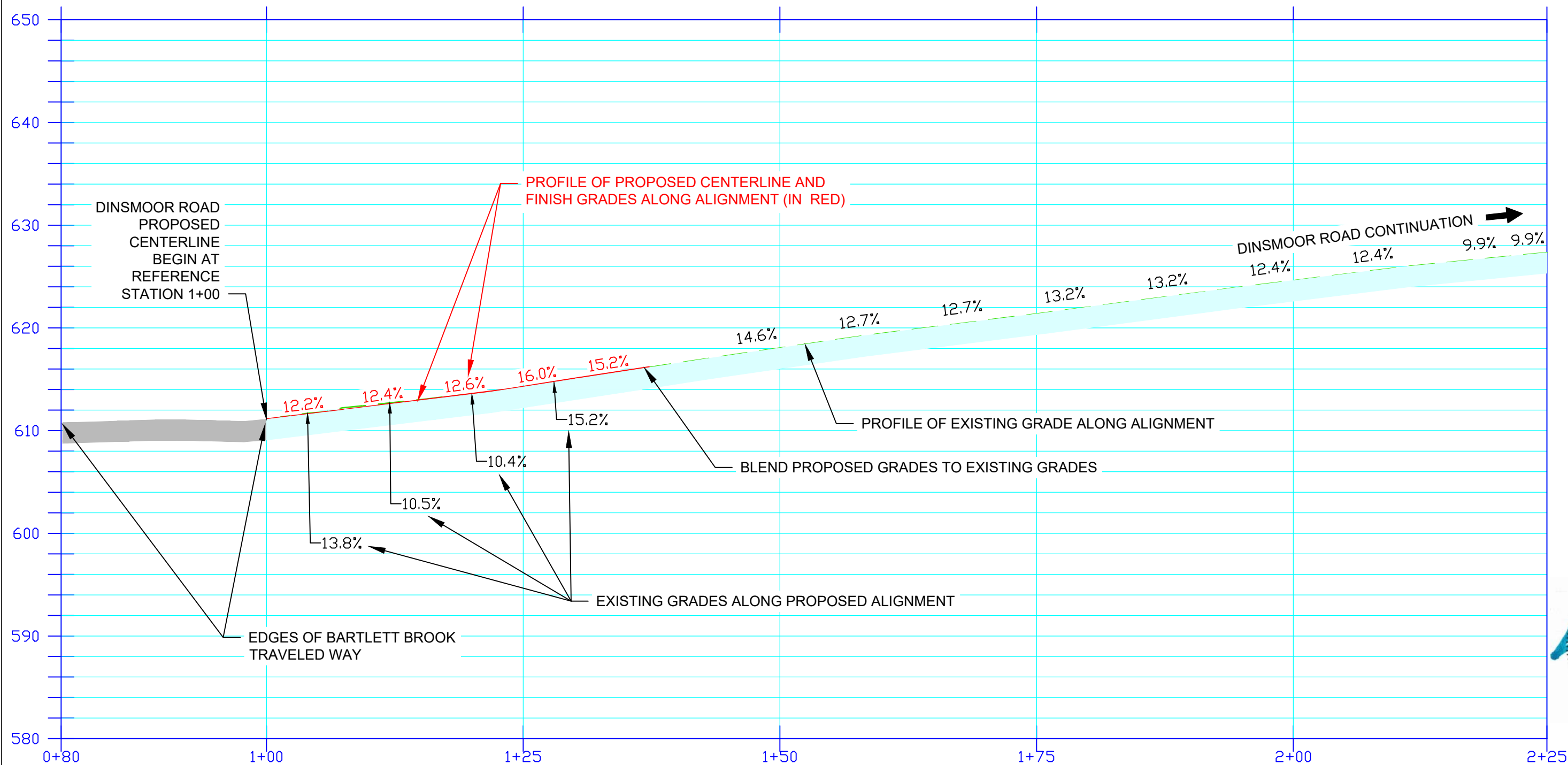
PROPOSED IMPROVEMENTS PLAN
SCALE: 1" = 10'

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SHEET TITLE:	PROPOSED IMPROVEMENTS PLAN - OPTION "A"						

DINSMOOR ROAD
SHARED ACCESS
Bartlett Brook
Pomfret, Vermont



MACKENZIE
Engineering and Construction
1313 VT Route 110, So. Royalton, VT 05068
Mobile No.: 802 - 356 - 5617
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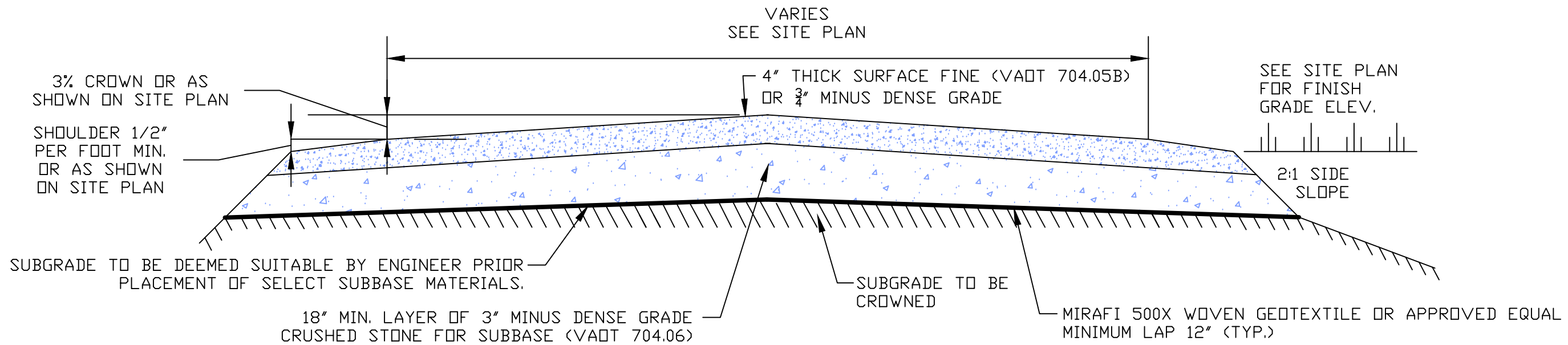
**DINSMOOR ROAD
PROPOSED CENTERLINE ALIGNMENT PROFILE
SCALE: 1" = 10' (HORIZONTAL & VERTICAL)**

PROJECT TITLE:	SHARED ACCESS ENTRANCE TO BARTLETT BROOK
SHEET TITLE:	PROPOSED CENTERLINE PROFILE
PROJECT NO.:	2304
SHEET NO.:	C2.10
DATE:	6-13-23

DINSMOOR ROAD
SHARED ACCESS
Bartlett Brook
Pomfret, Vermont



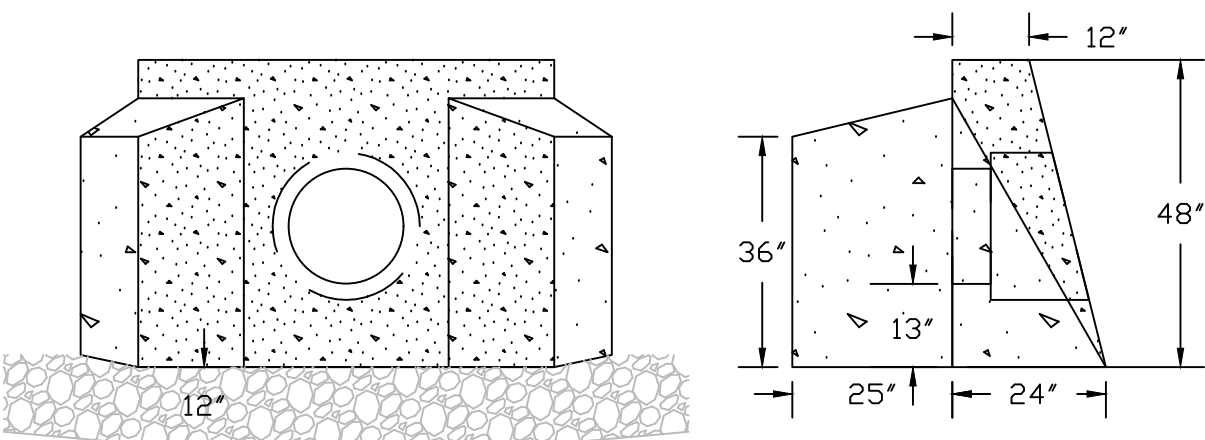
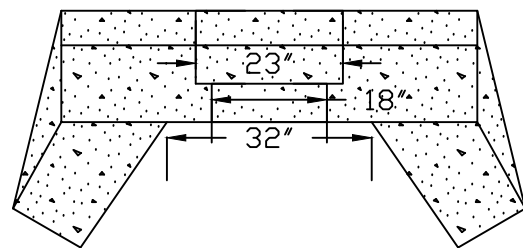
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1313 VT Route 110, So. Royalton, VT 05068
Mobile No.: 802 - 356 - 5617
Email: ian@mack-eng.com



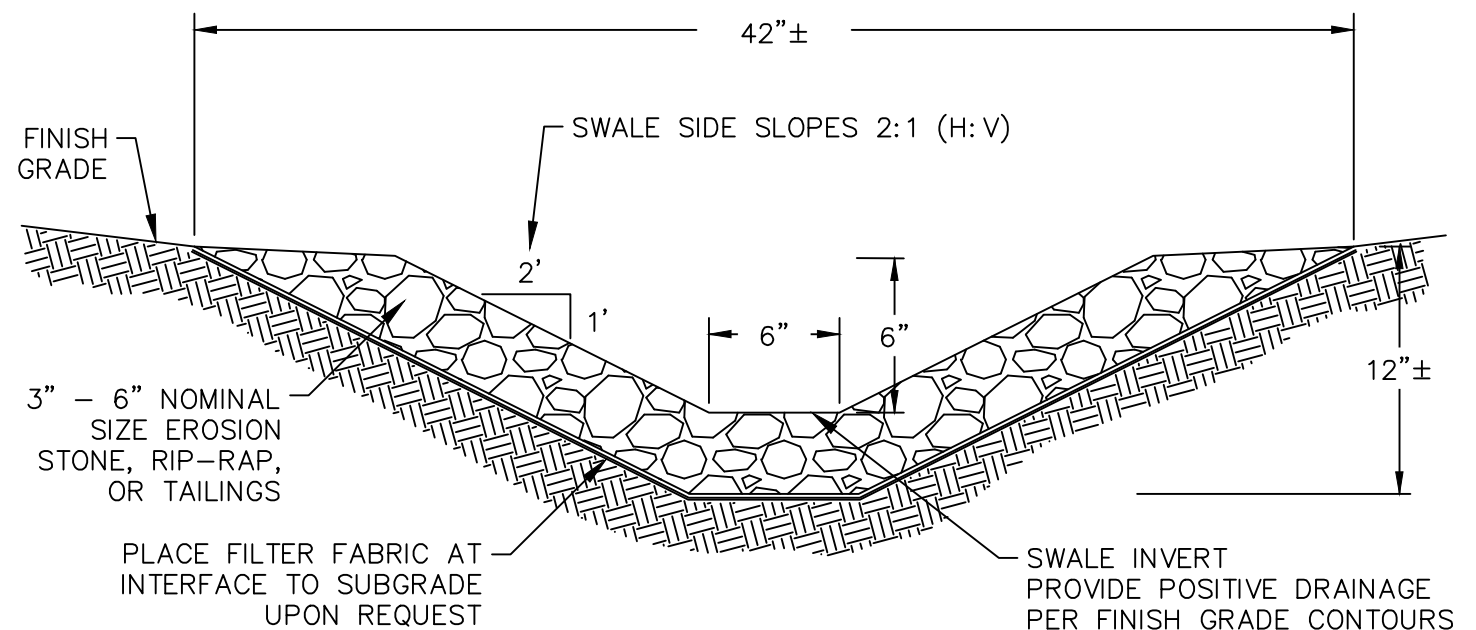
**SHARED ACCESS BOX-CUT AND GRADING
CROSS SECTION DETAIL
NOT TO SCALE**

- FINAL ROADWAY CONSTRUCTION WILL REQUIRE:
- REMOVAL OF ALL CONTAMINATED CRUSHED GRAVEL BASE MATERIALS CAUSED BY CONSTRUCTION ACTIVITIES.
 - REFILL, REPLACEMENT AND COMPACTION OF THE SUBBASE TO PROVIDE A MINIMUM DEPTH AS SHOWN.
 - GRANULAR MATERIALS SHALL BE COMPACTED TO 95% OF THE MODIFIED PROCTOR DENSITY.

NOTE:
 PRECAST CONCRETE HEADWALL AS SHOWN IS BY WEED PRECAST OF WHITE RIVER JCT., VT. CONSTRUCTED OF 4000 PSI COMPRESSIVE STRENGTH CONCRETE BASED ON 28 DAY CURE WITH STANDARD FIBERMESH AT 1.5 PCY. 6X6X10 GA. WELDED WIRE MATT WITH #3 AND #4 REBAR REINFORCEMENT TO BE PROVIDED ACCORDING TO MANUFACTURER'S SPECIFICATIONS. JOINT SEAL OR MORTAR CULVERT PIPE TO STRUCTURE ACCORDING TO MANUFACTURER'S SPECIFICATIONS. PRECAST HEADWALL IS 5,800# PER MANUFACTURER. PLACE ON 12" COMPACTED 1 1/2" CLEAN CRUSHED STONE PAD WITH MIRAFI 160N FABRIC AT SUBGRADE INTERFACE. BACKFILL WITH CLEAN GRANULAR BACKFILL.



**PRECAST CONCRETE HEADWALL DETAIL
NOT TO SCALE**



**STONE-LINED SWALE DETAIL
NOT TO SCALE**



PROJECT TITLE:	SHARED ACCESS ENTRANCE TO BARTLETT BROOK	PROJECT NO.:	2304
SHEET TITLE:	MISCELLANEOUS DETAILS	SHEET NO.:	C2.20
DATE:	6-13-23		

DINSMOOR ROAD
 SHARED ACCESS
 Bartlett Brook
 Pomfret, Vermont

DOCUMENT NOT VALID
 WITHOUT ORIGINAL
 STAMP & SIGNATURE

Mackenzie
 Engineering and Construction
 1313 VT Route 110, So. Royalton, VT 05068
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IMG_1848 7-29-23



IMG_1854 July 29, 2023



IMG_1869 7-29-23

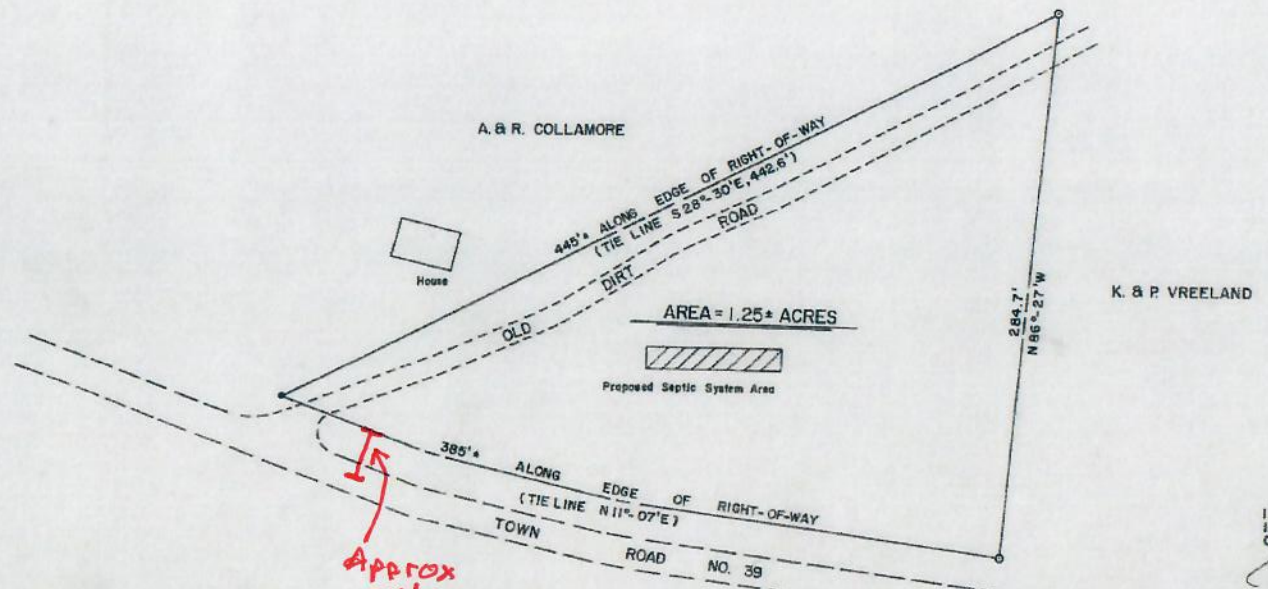
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Exhibit 6. MapBk2 Pg136 Mann land boundary 10-31-84



NOTES

- BEARINGS ARE WITH RESPECT TO MAGNETIC NORTH
- PROPERTY IS SUBJECT TO ALL EXISTING EASEMENTS AND RIGHTS-OF-WAY-OF RECORD
- BOUNDARIES BASED ON PHYSICAL EVIDENCE, A MAP BY BRUNO ASSOC. FOR R.L. & A.G. COLLAMORE DATED MARCH 1978, AND DEEDS RECORDED IN POMFRET LAND RECORDS



Approx 24' CENTER BOR TO EDGE OF TOWN ROW/MANN LAND BOUNDARY LINE

LEGEND

○	IRON PIN SET
•	BOUNDARY POINT
---	ROADWAY

I DO HEREBY CERTIFY THIS MAP AND INFORMATION TO BE SUBSTANTIALLY CORRECT VI. L. S. 485

BRUNO ASSOCIATES ENGINEERS, PLANNERS, SURVEYORS WOODSTOCK, VERMONT	SURVEYED DE & KH
	DESIGNED
PARCEL OF LAND IN POMFRET WINDSOR COUNTY VERMONT TO BE CONVEYED BY: K. & P. VREELAND TO: ANDREW MANN	DRAWN CF
	CHECKED SH
	SCALE 1" = 40'
	DATE OCT. 31, 1984
REVISIONS	

Pomfret Town Clerk's Office: Received for record on July 3, 1984 at 4:45 P.M. attest: April S. Huntington, Town Clerk